

Prince George's County

# Fall Service Change

October 5, 2023



Prepared by:





# Overview

# Overview

- Service changes were identified for both Fall 2023 and Spring 2024
- As of October 5, this presentation reviews only proposed Fall changes – Spring changes are pending further discussion
- Existing TheBus revenue hours: **240,144**
- Proposed TheBus revenue hours (December 2023): **245,039**
  - Increase of **2.0%**
- Existing TheBus peak vehicle requirement: **72**
- Proposed TheBus peak vehicle requirement: **73**



# Types of Changes

Fall 2023 / Spring 2024

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# Adjusted Runtimes

- Based on operator feedback and OTP measured through Swiftly, runtimes were adjusted upward as appropriate
  - Almost every route's runtime is being adjusted
  - In general, runtime adjustments are not called out in this presentation unless particularly notable

# Improved LOS

- For productive routes in the evening (high volumes of riders per revenue hour), span of service was extended from 6:00 p.m. to 8:00 p.m.
  - 16, 18, 30, 32
  - These routes also have high volumes of riders per vehicle trip
  - Exception: 51X scores high on this metric, but only in the morning, so span was not extended
- Increased the frequency of Route 18 to every 20 minutes based on overall productivity
- For some routes, add a revenue trip before 6:00 a.m. or after the end of the span to minimize long deadheading
  - Does not apply to all routes with a long deadhead – e.g., an early AM run of the 36 toward Brandywine is unlikely to pick up a significant volume of passengers

# Decreased LOS

- Reduced Saturday service on routes with relatively low productivity on weekends compared to weekdays
  - 13A – cut Saturday service
  - 21 – short turn on Saturdays at Largo (no service between Largo and Upper Marlboro), should be noted PGCC is not open on Saturdays
- Reduced LOS based on a reduction in productivity throughout the day
  - 51X – operate with 3 vehicles in the AM peak, 2 or 3 vehicles from 9 a.m. to 2 p.m., 2 vehicles after 2 p.m.
- Low OTP cut into operator recovery time, so reduced frequency to allow operators to have their full recovery with an adjusted (longer) runtime.
  - 34 – drop from 15 to 20 minutes in the peak, 30 to 45 in the midday

# Realignments

- Routes were realigned either because of operator feedback or to sync with the BBNR Year One Network.  
*(Very small realignments not listed on this slide)*
  - 14 – cut service on Route 1
  - 21 – realign to Brightseat Road
  - 21X – realign to serve Largo Metro
  - 24 – swap Ritchie Road/Hampton Park Blvd segments with 26
  - 26 – swap Ritchie Road/Hampton Park Blvd segments with 26 and realign onto Apollo, Mercantile, Campus Way in Largo
  - 35 – bidirectional service on Allentown Road instead of Brinkley Road
  - 53 – remove service from the Equestrian Park & Ride



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# Note on BBNR Year One Network

- The relationship between the proposed Fall Service Change and the analogous route in the BBNR Year One Network is noted on the following slides
  - We recognize that the BBNR Year One Network is still in flux
- In some cases, the BBNR Year One Network may need to be adjusted to conform to a service change (typically a service improvement) that has been identified for the Fall Service Change



# Route by Route

# 11

Fall

- Only runtime adjusted
- This route is **covered by Metrobus** in the BBNR Year One Network

Existing Rev Hrs	Proposed Rev Hrs
5,684	6,409

# 13A

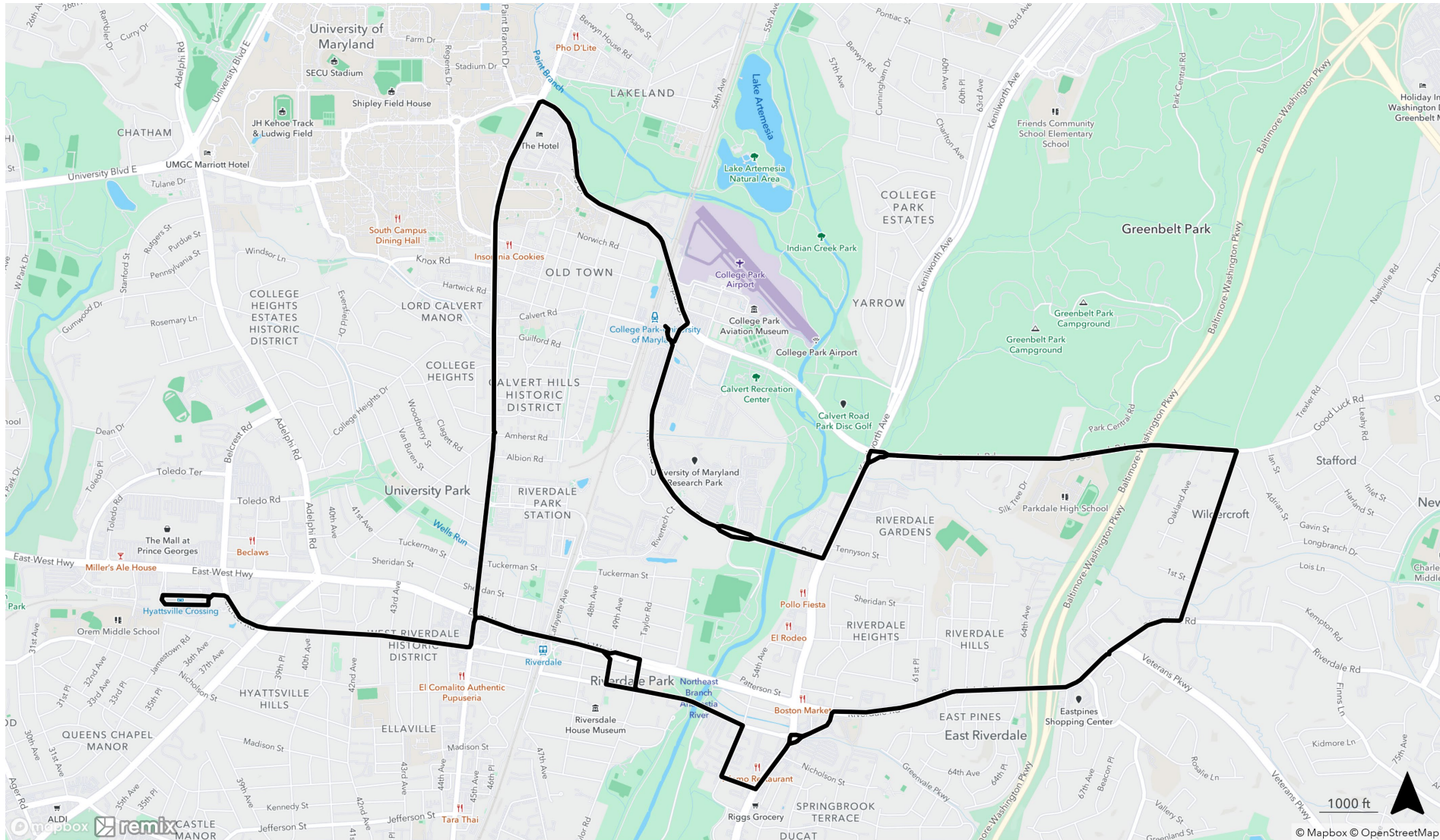
Fall

Spring

- Fall: Cut Saturday service
- Spring: Eliminate and replace with microtransit (pending further discussion)
- This route is **eliminated** in the BBNR Year One Network

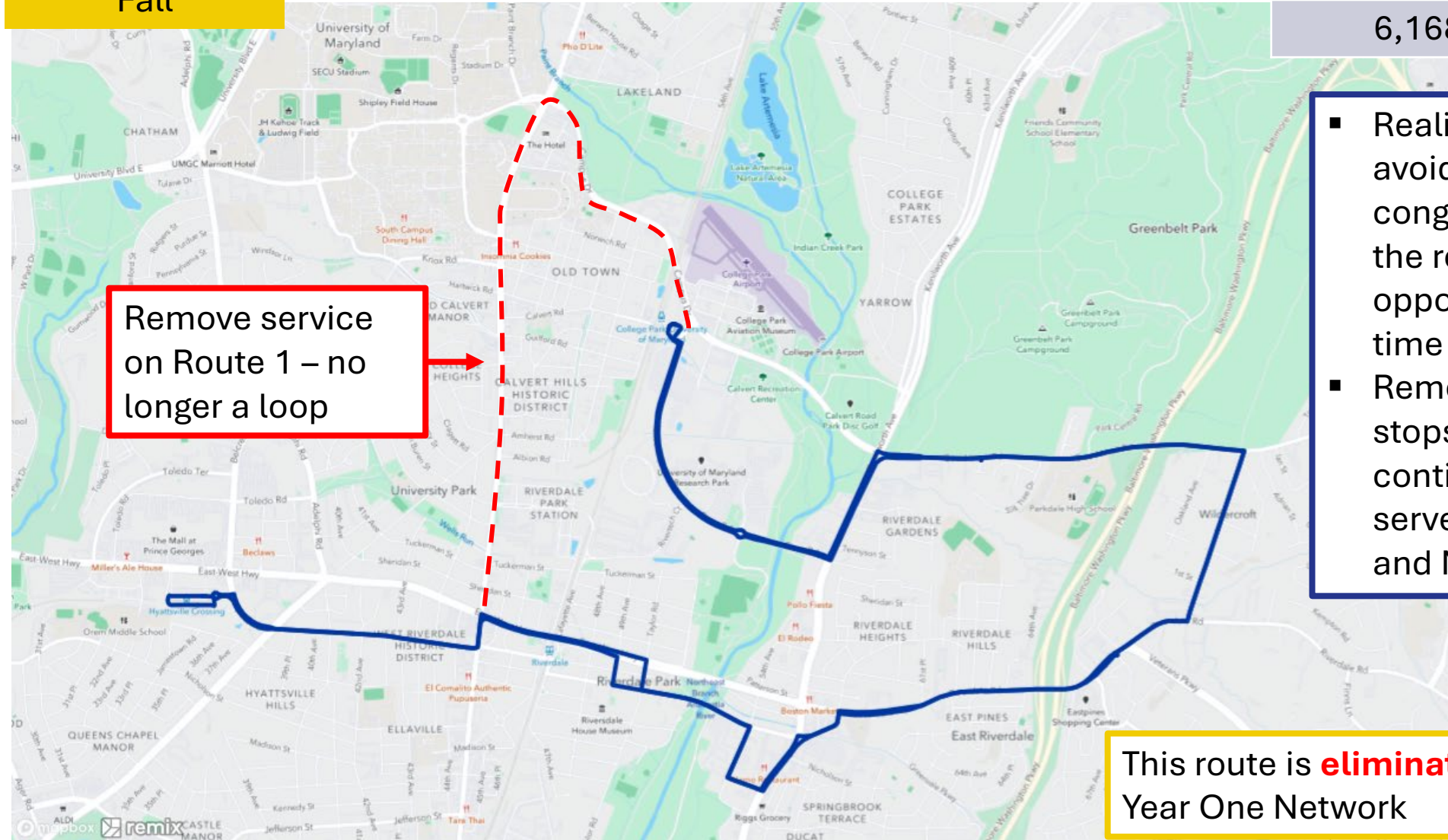
Existing Rev Hrs	Proposed Rev Hrs (Fall)
3,513	3,035

# 14 - Existing



# 14 - Proposed

Fall



Existing Rev Hrs	Proposed Rev Hrs
6,168	6,171

- Realigned route avoids Route 1 congestion, giving the route an opportunity to run on time
- Removed stops/segments continue to be served by Route 17 and Metrobus

This route is **eliminated** in the BBNR Year One Network

# 15X

Spring

- Fall: Adjust runtimes only
- Spring: Eliminate (pending further discussion)
- This route is **eliminated** in the BBNR Year One Network

Existing Rev Hrs	Proposed Rev Hrs (Fall)
5,100	5,202

# 16

Fall

- Extend span to 8:00 p.m. Mon – Sat
- This route has **additional adjustments** in the BBNR Year One Network

Existing Rev Hrs	Proposed Rev Hrs
14,079	14,906



# 17

Fall

Existing Rev Hrs	Proposed Rev Hrs
9,306	11,351

- Lengthen runtime on Route 1 (with different runtimes in different periods based on past performance)
- Add trips from Mount Rainier in the AM/to Mount Rainier in the PM to minimize long deadhead
- This route is **covered by Metrobus** in the BBNR Year One Network

# 18

Fall

- Service extended to 8:00 p.m.
- Service runs every 20 mins
- Add some early AM trips and late PM trips between Cheverly and Takoma-Langley to minimize long deadhead
- This route **reflects** the BBNR Year One Network

Existing Rev Hrs	Proposed Rev Hrs
21,506	32,653

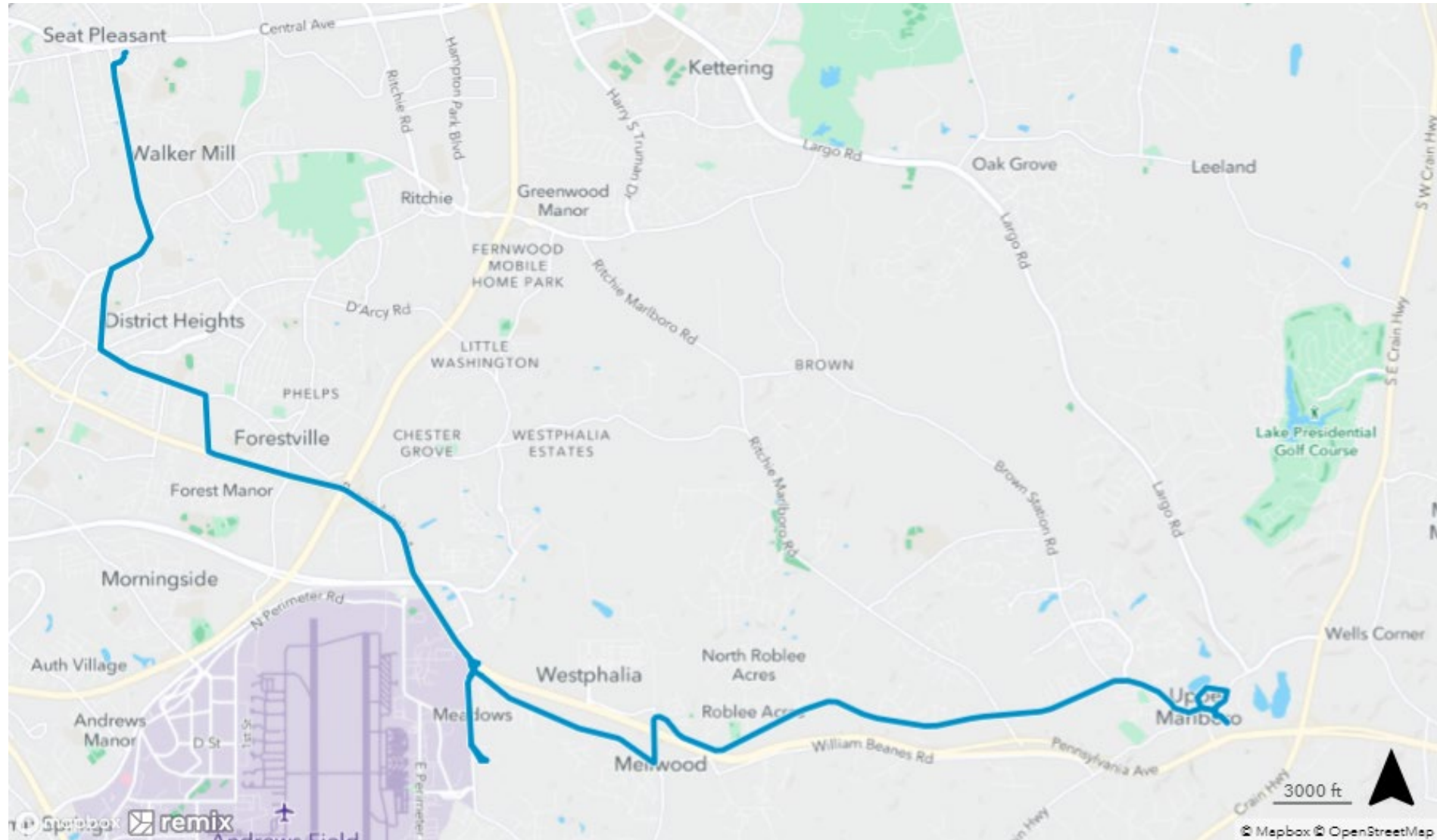
# 19

Spring

Existing Rev Hrs	Proposed Rev Hrs (Fall)
7,957	7,388

- Fall: Adjust runtimes only
- Spring: Eliminate and replace with microtransit (pending further discussion)
- This route is **eliminated** in BBNR Year One

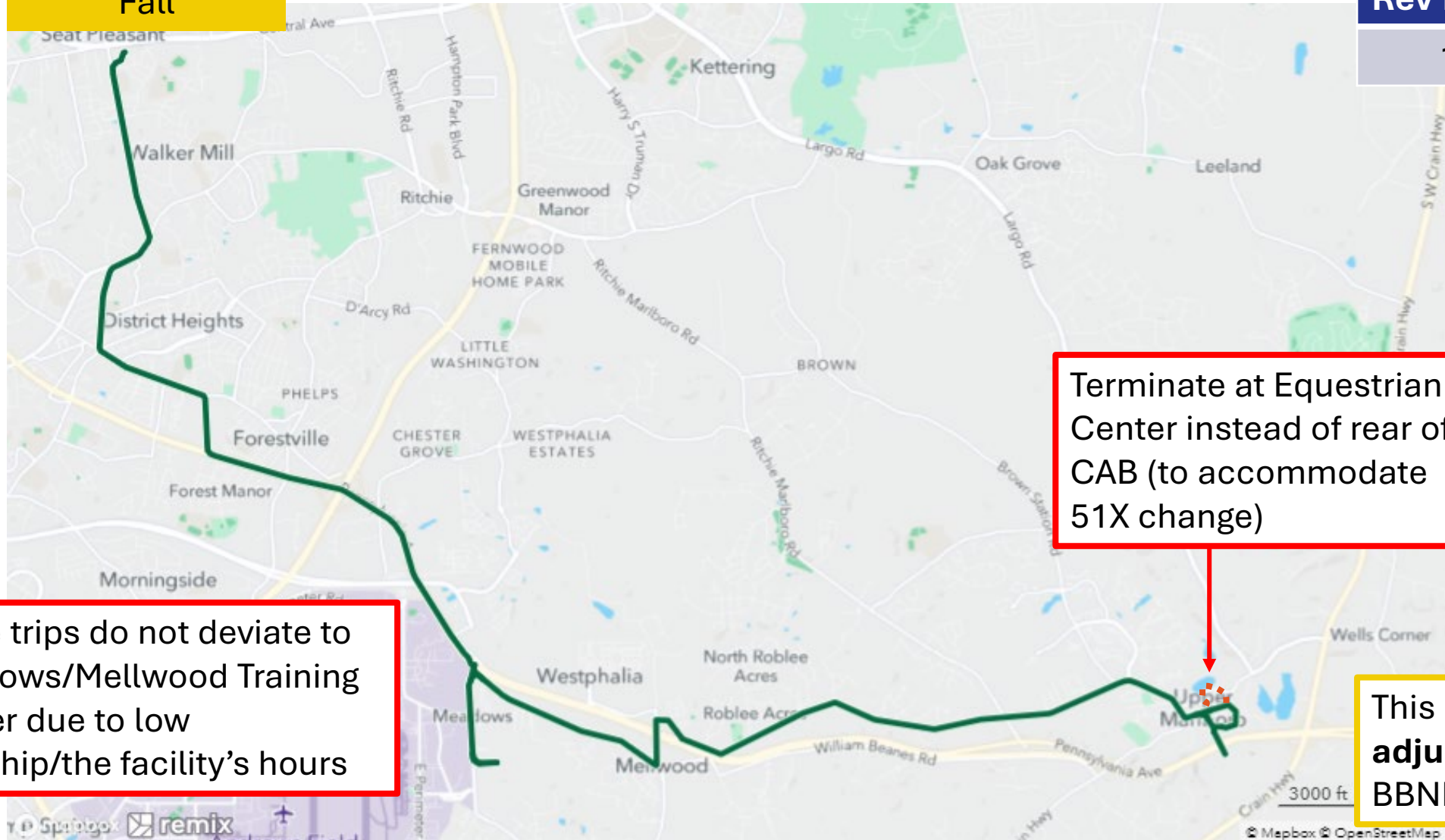
# 20 - Existing



# 20 - Proposed

Fall

Existing Rev Hrs	Proposed Rev Hrs
14,925	14,253

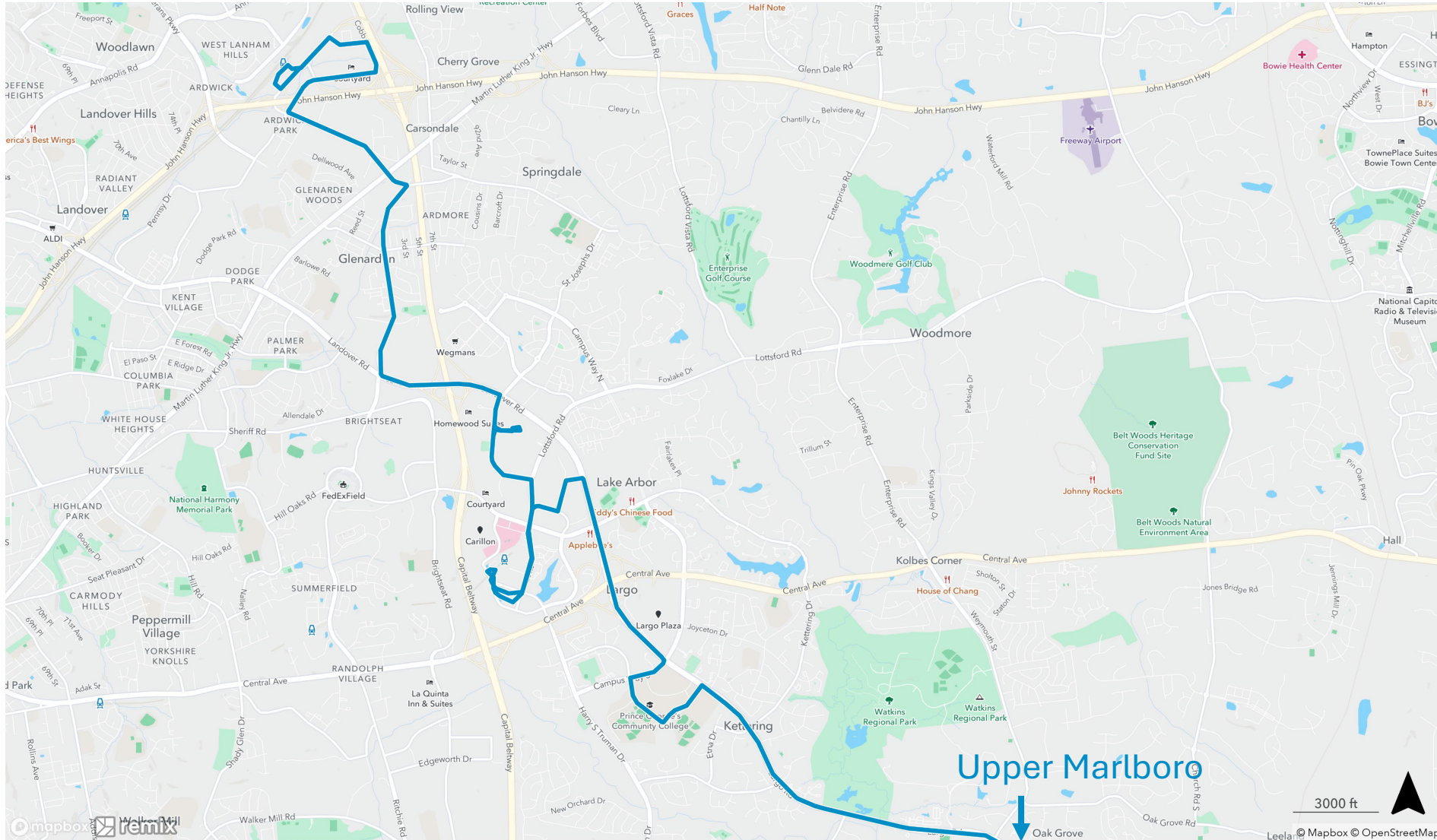


Terminate at Equestrian Center instead of rear of CAB (to accommodate 51X change)

Some trips do not deviate to Meadows/Mellwood Training Center due to low ridership/the facility's hours

This route has **additional adjustments** in the BBNR Year One Network

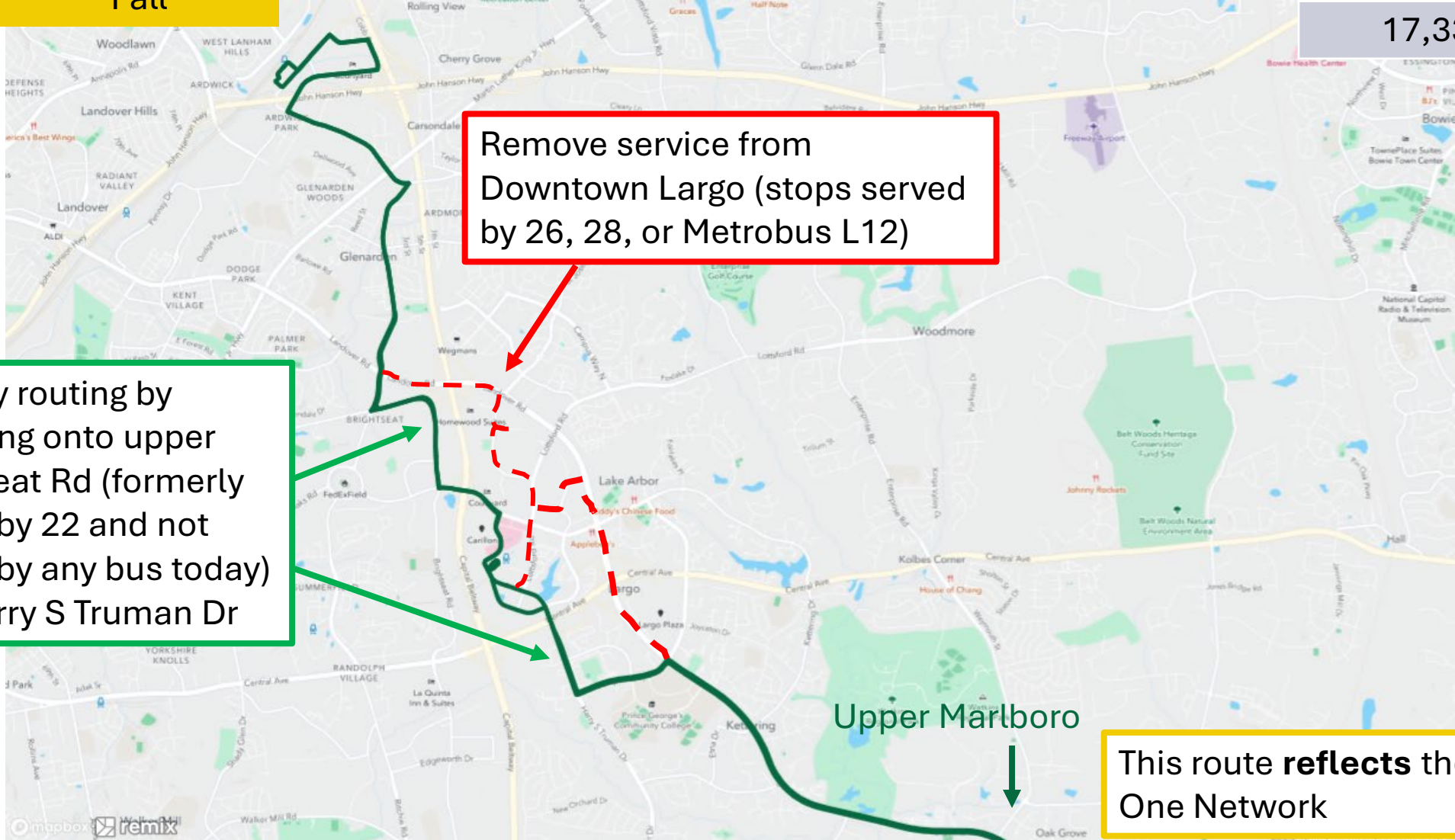
# 21 - Existing



# 21 – Proposed (Weekday)

Fall

Existing Rev Hrs	Proposed Rev Hrs
17,334	14,595



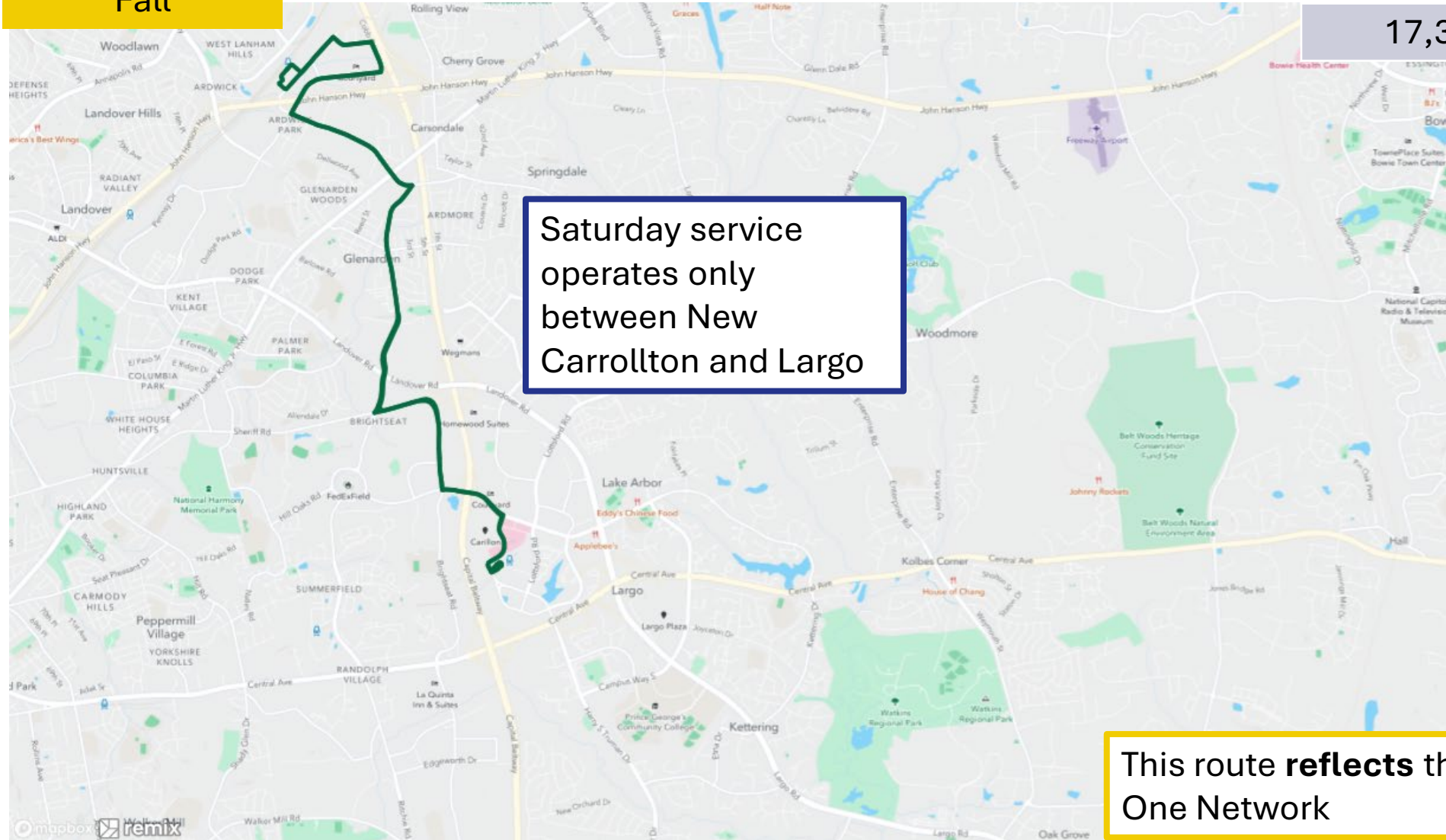
Simplify routing by realigning onto upper Brightseat Rd (formerly served by 22 and not served by any bus today) and Harry S Truman Dr

Remove service from Downtown Largo (stops served by 26, 28, or Metrobus L12)

This route **reflects** the BBNR Year One Network

# 21 – Proposed (Saturday)

Fall



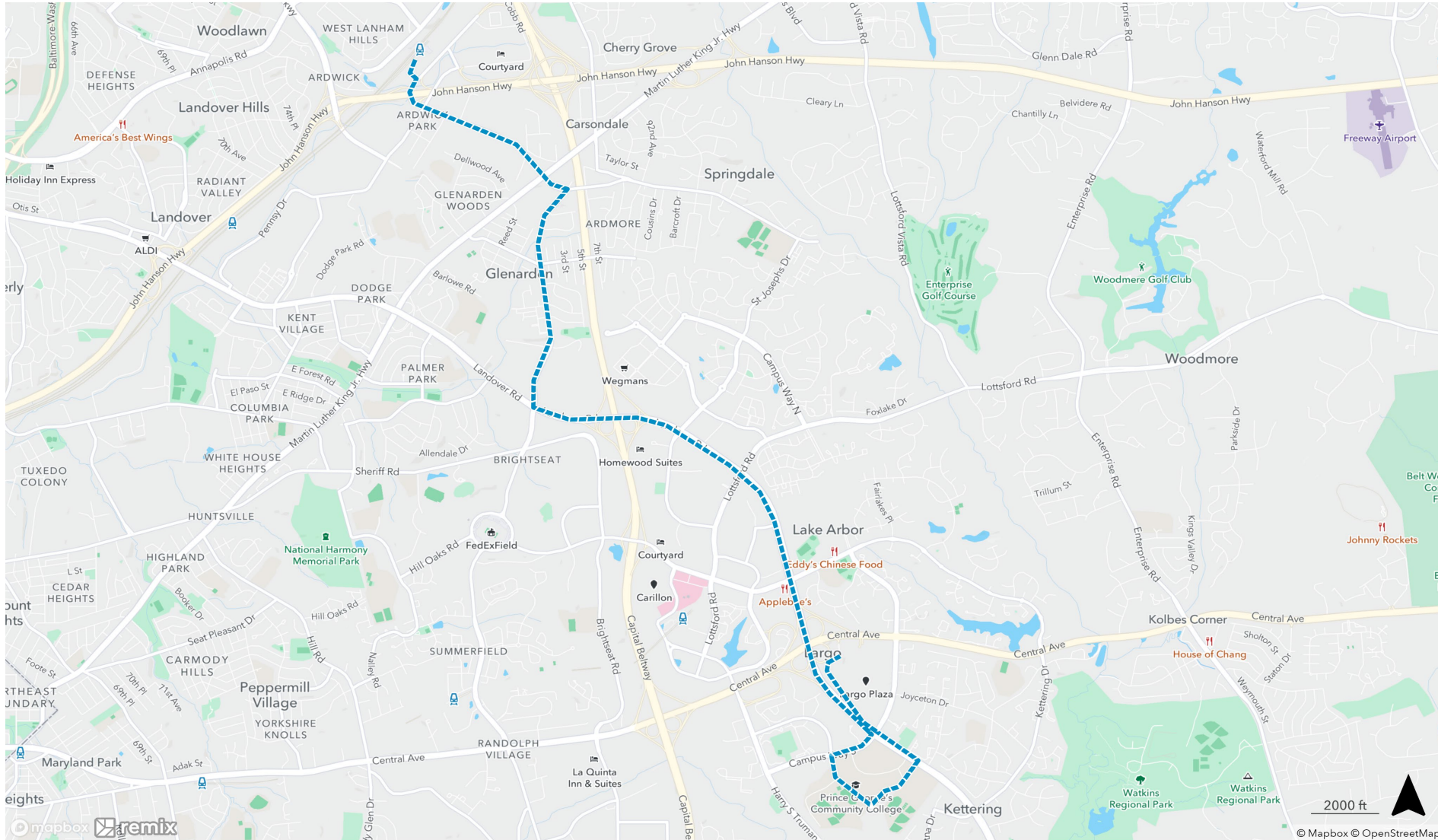
Existing Rev Hrs	Proposed Rev Hrs
17,334	14,595

Saturday service operates only between New Carrollton and Largo

This route **reflects** the BBNR Year One Network



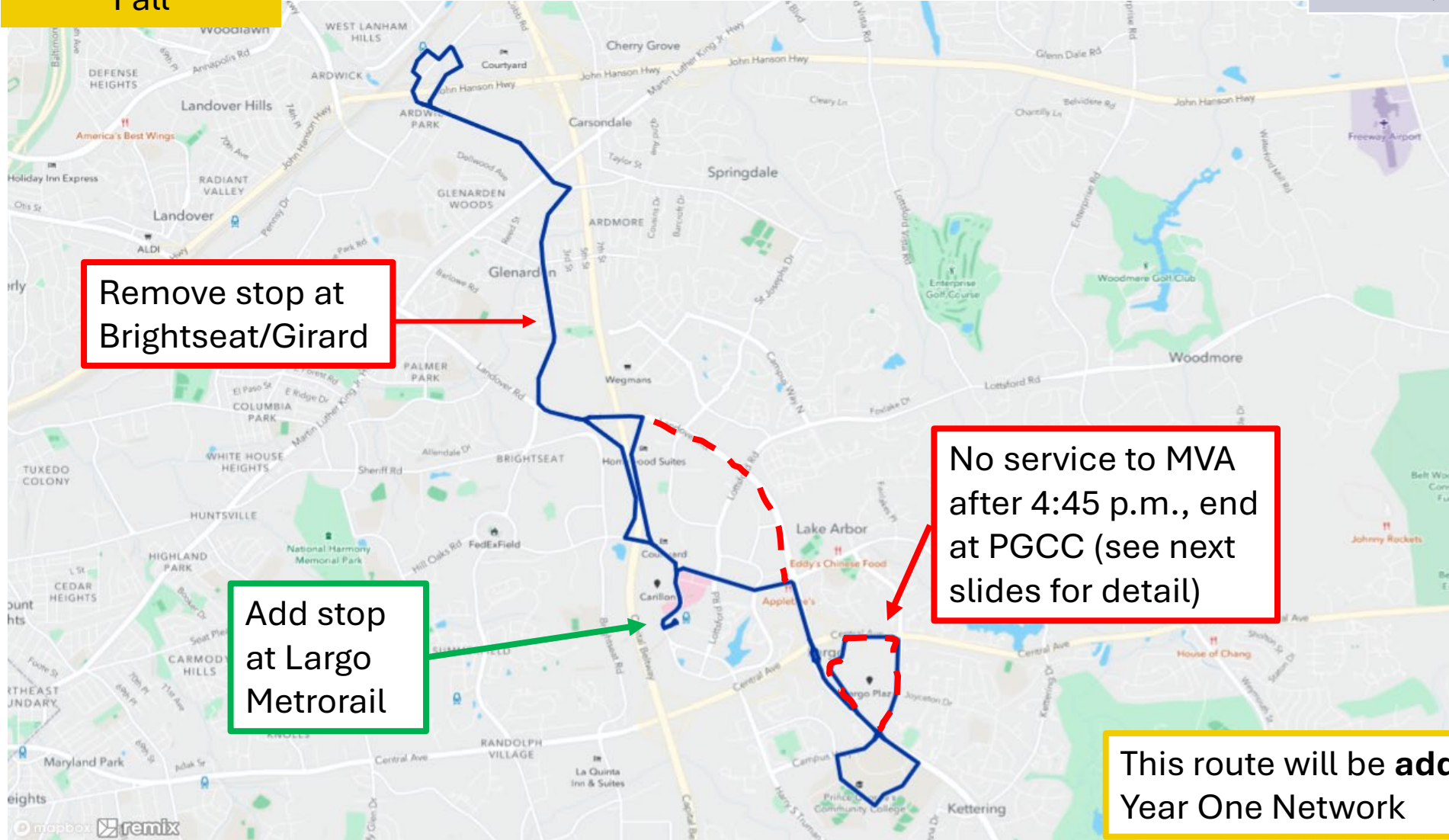
# 21X – Existing



# 21X – Proposed

Existing Rev Hrs	Proposed Rev Hrs
4,888	5,071

Fall



Remove stop at Brightseat/Girard

Add stop at Largo Metrorail

No service to MVA after 4:45 p.m., end at PGCC (see next slides for detail)

This route will be **added to the BBNR Year One Network**

# 21X – Existing (Inbound Detail)

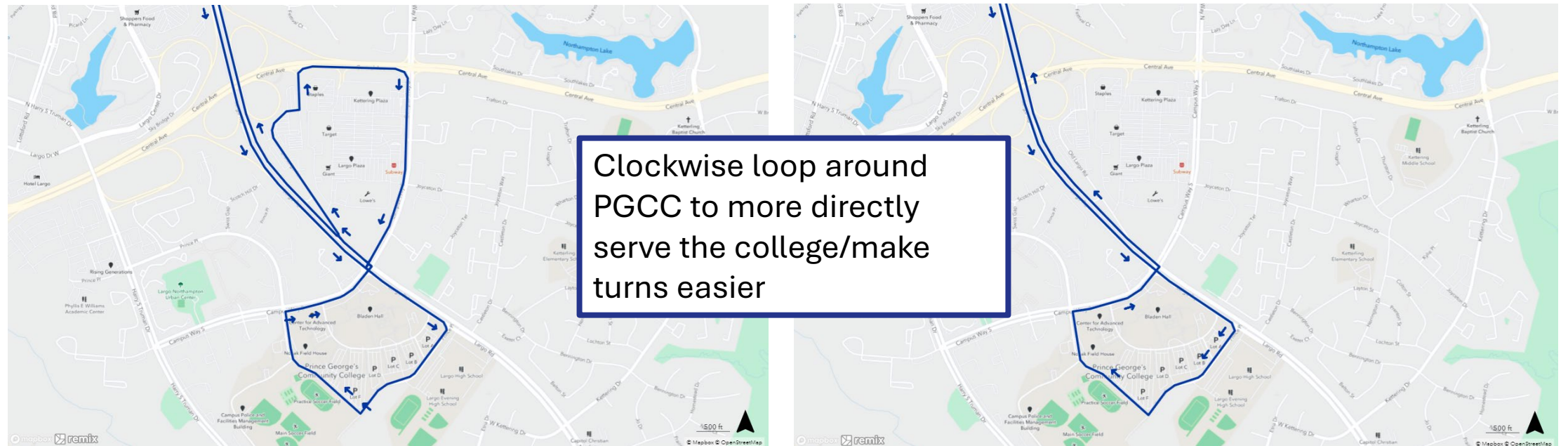


# 21X - Proposed

Fall

## Main Pattern

Short Turn (after 4:45 p.m.  
when MVA is closed)



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# 23

Spring

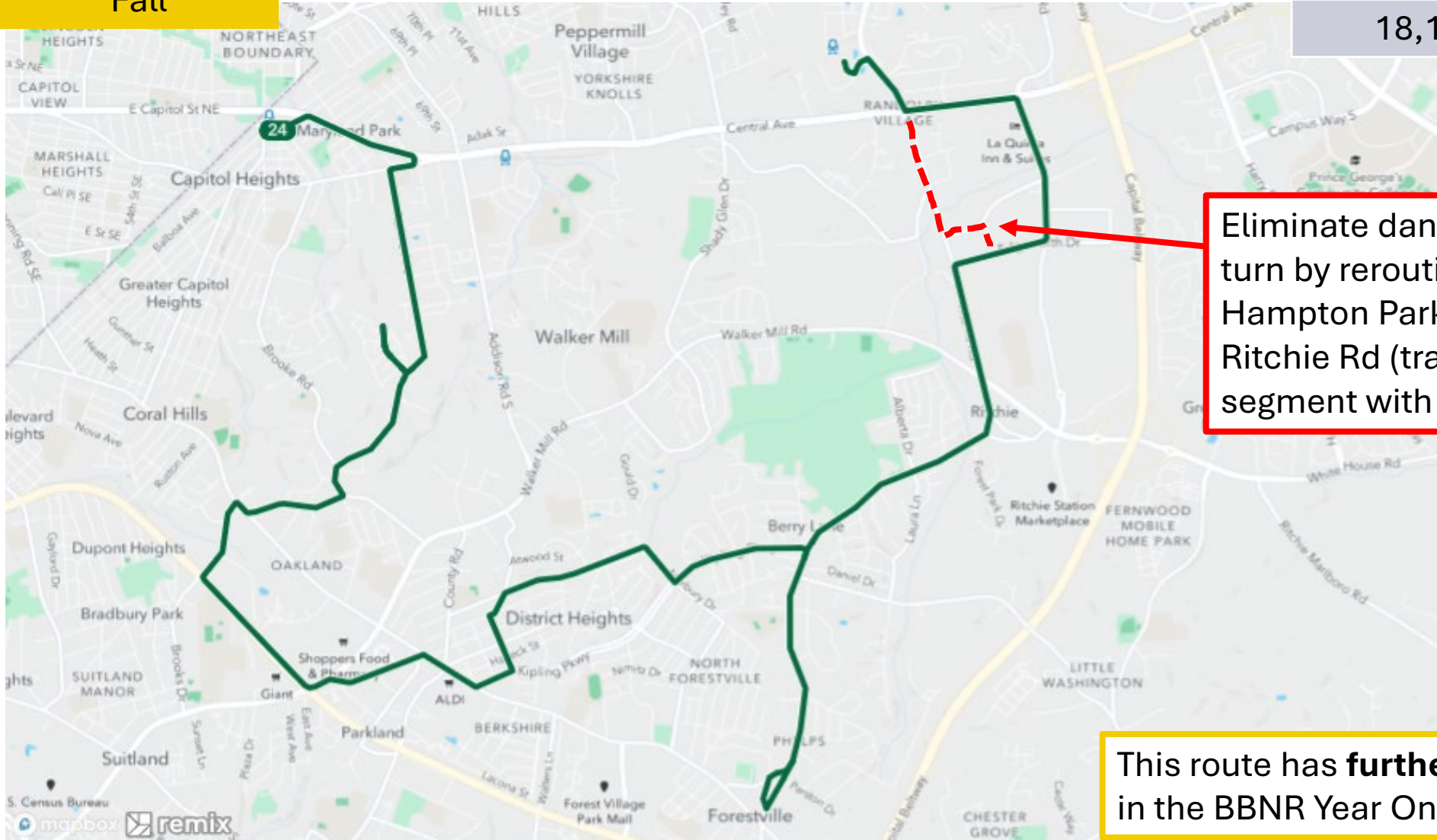
- Fall: No changes
- Spring changes pending further discussion



# 24 - Proposed

Fall

Existing Rev Hrs	Proposed Rev Hrs
18,116	15,890



Eliminate dangerous turn by rerouting from Hampton Park Blvd to Ritchie Rd (trade segment with 26)

This route has **further adjustments** in the BBNR Year One Network





# 26 – Proposed

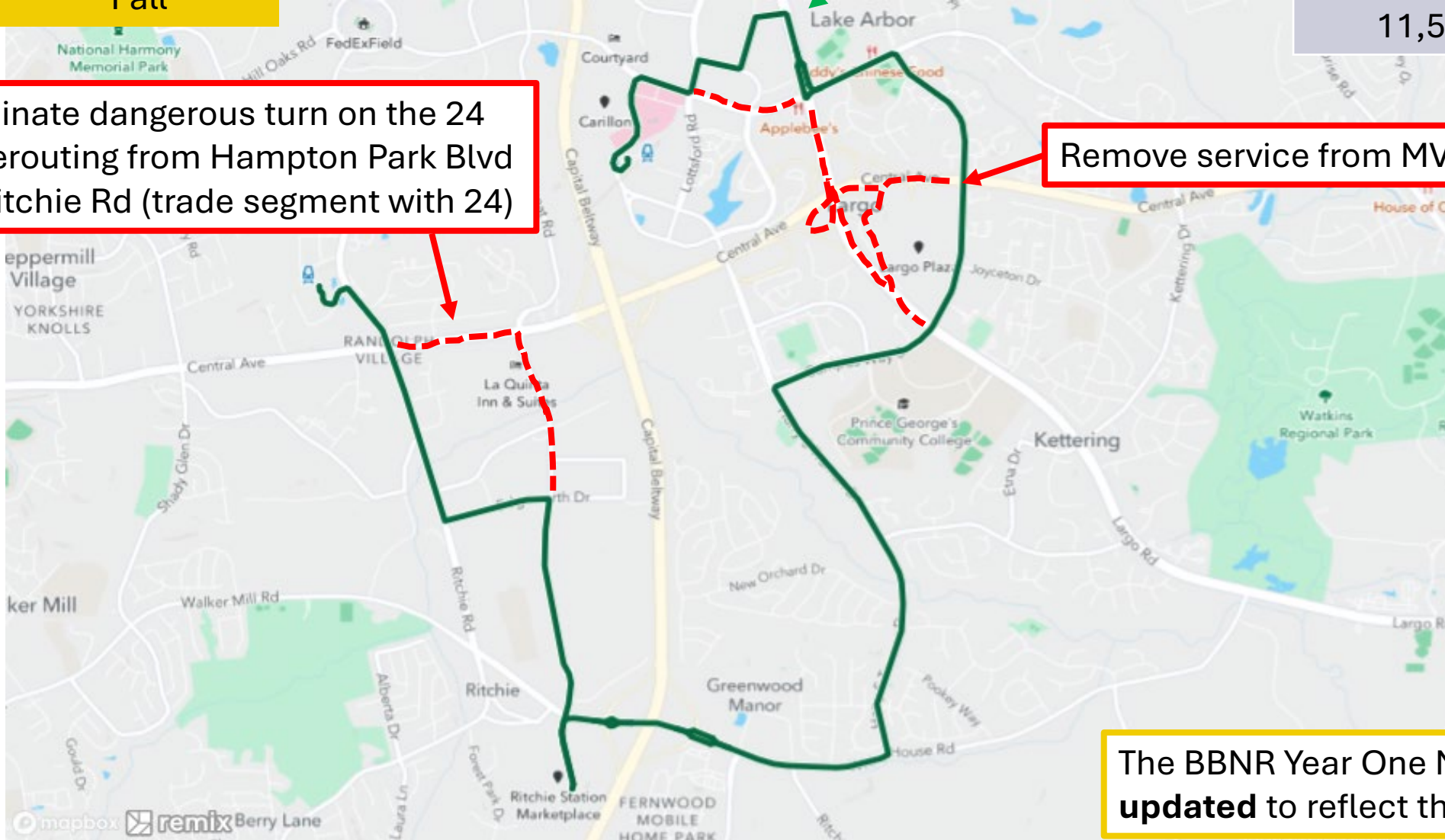
Fall

Reroute onto Lottsford, Apollo, Mercantile, Technology, Campus Way (segment from 21)

Eliminate dangerous turn on the 24 by rerouting from Hampton Park Blvd to Ritchie Rd (trade segment with 24)

Remove service from MVA

Existing Rev Hrs	Proposed Rev Hrs
11,515	13,925



The BBNR Year One Network **will be updated** to reflect these changes

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# 28

- Fall: No changes
- Spring changes pending further discussion

# 30

Fall

- Extend span to 8:00 p.m. on weekdays

Existing Rev Hrs	Proposed Rev Hrs
13,220	14,617

This route **reflects** the BBNR Year One Network

# 32

Fall

- Extend span to 8:00 p.m. on weekdays

Existing Rev Hrs	Proposed Rev Hrs
16,792	17,211

This route has **additional adjustments** in the BBNR Year One Network

# 33

Fall

- Fall: Slight runtime adjustment
- Spring changes pending further discussion

Existing Rev Hrs	Proposed Rev Hrs
10,656	10,827

# 34

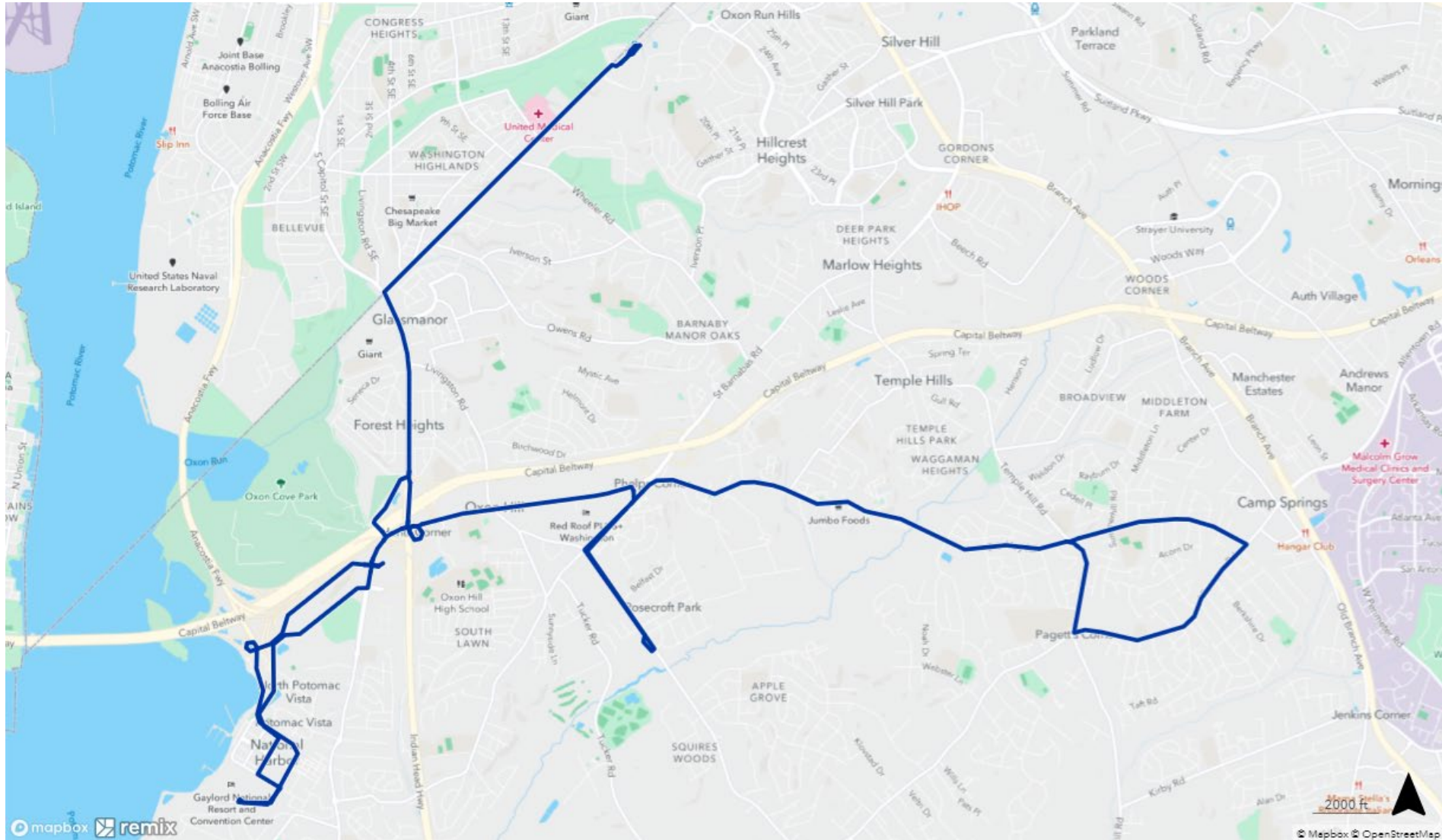
Fall

- Decrease frequency from 15 to 20 minutes in AM and PM Peak and 30 to 45 minutes in midday to improve OTP
- Lengthen runtime without adding a vehicle

Existing Rev Hrs	Proposed Rev Hrs
6,001	3,802

The BBNR Year One Network **will be updated** to reflect these changes

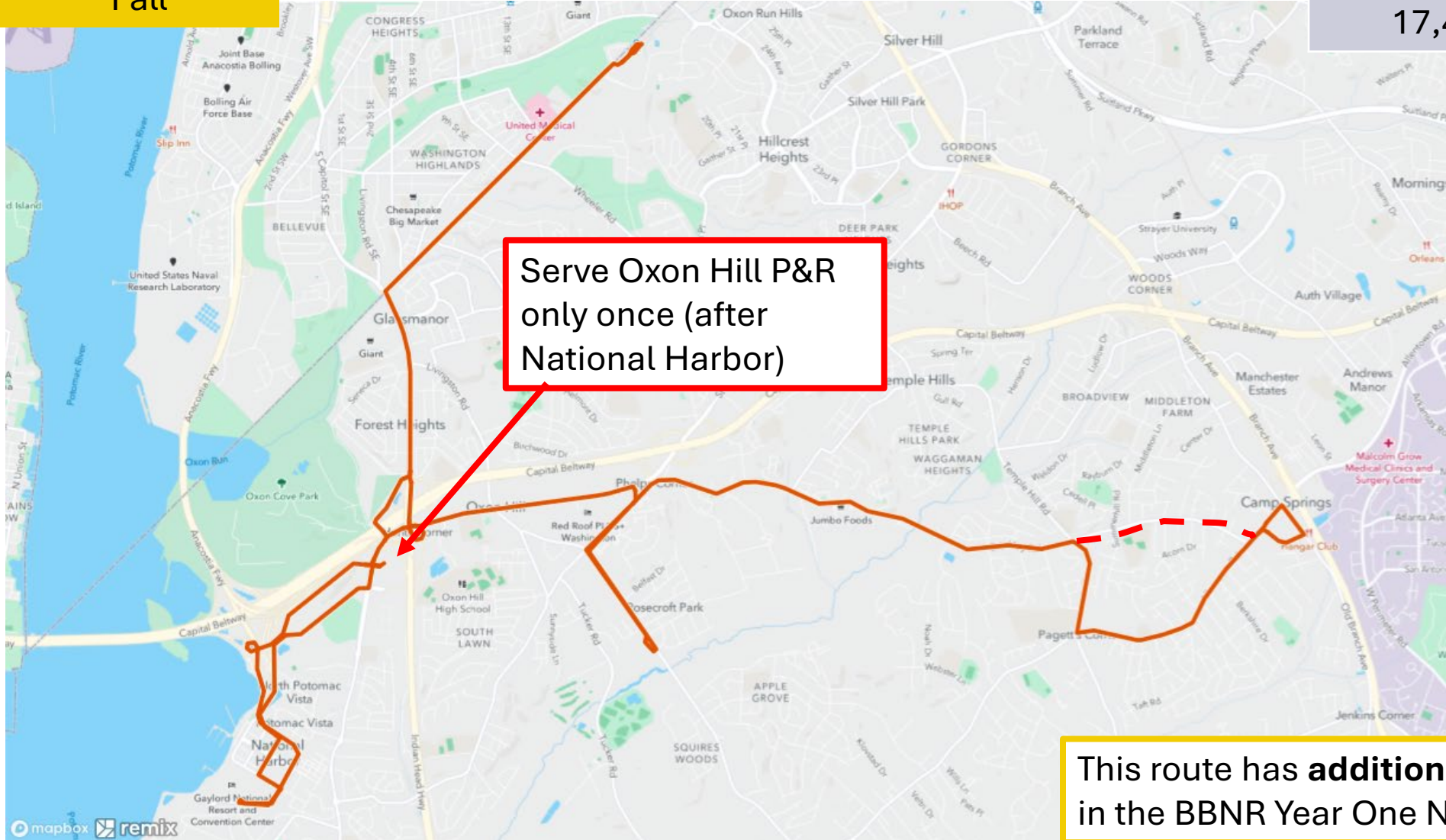
# 35 - Existing



# 35 - Proposed

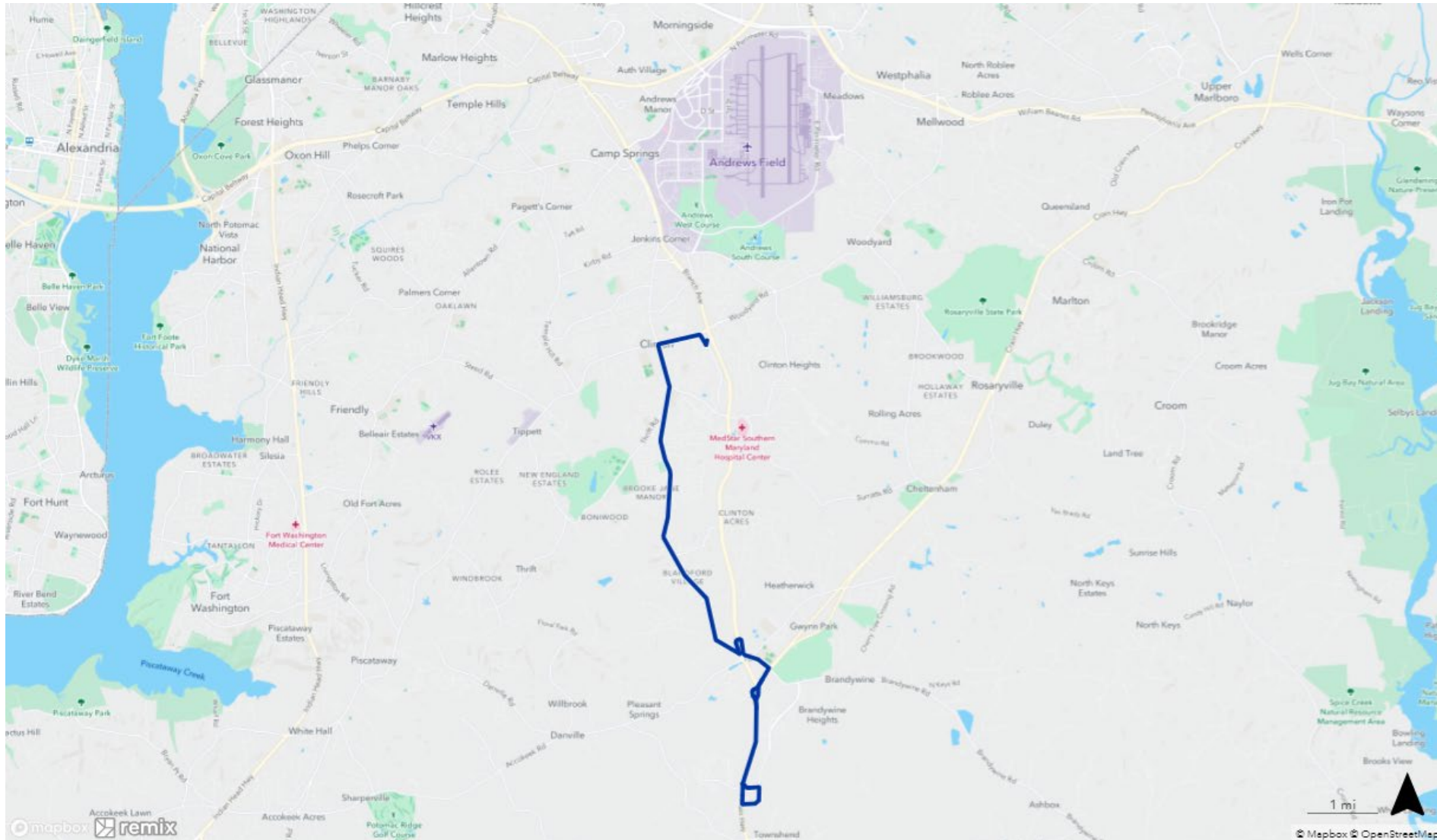
Fall

Existing Rev Hrs	Proposed Rev Hrs
17,438	15,500





# 36 - Existing

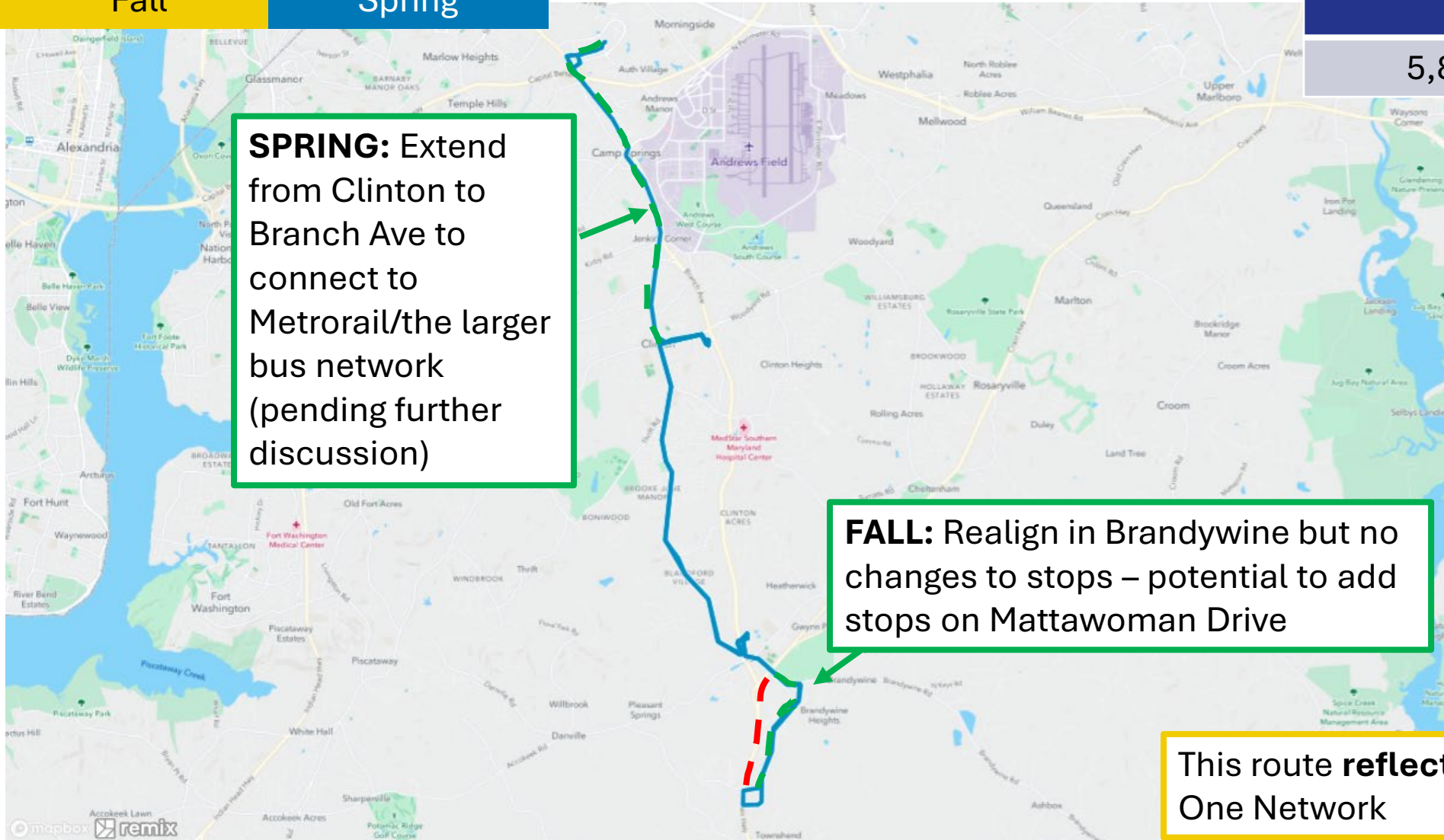


# 36 - Proposed

Fall

Spring

Existing Rev Hrs	Proposed Rev Hrs (Fall)
5,819	6,592



**SPRING:** Extend from Clinton to Branch Ave to connect to Metrorail/the larger bus network (pending further discussion)

**FALL:** Realign in Brandywine but no changes to stops – potential to add stops on Mattawoman Drive

This route **reflects** the BBNR Year One Network

# 37

Fall

Spring

Existing Rev Hrs	Proposed Rev Hrs (Fall)
9,235	5,988

- Fall: Adjust runtimes, in particular at Wilson Towers
- Spring: Eliminate and replace with microtransit (pending further discussion)
- This route is **eliminated** in the BBNR Year One Network

This route is **eliminated** in the BBNR Year One Network

# 51X

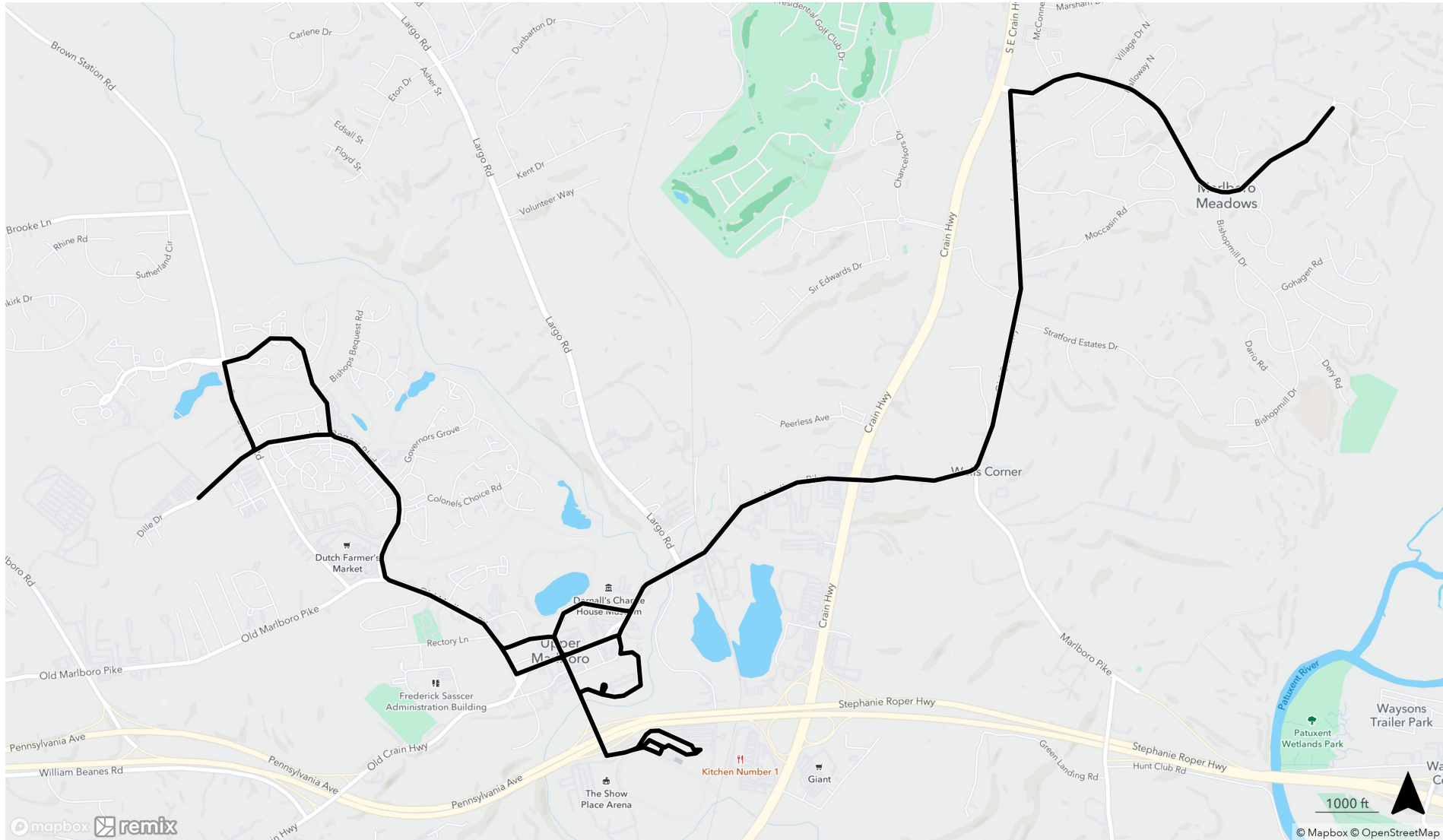
Fall

Existing Rev Hrs	Proposed Rev Hrs
9,235	5,988

- Route starts and ends at rear of CAB
- Remove all timepoints except rear of CAB, Courthouse, and Park & Ride Lot 1
- 6 a.m. to 9 a.m.: service every 5 minutes using 3 buses
  - Runtime of 9 minutes, recovery time of 6 minutes
  - Minimizing the number of timepoints should make it easier to meet these runs
- 9 a.m. to 2 p.m.: service every 7.5 minutes using 3 buses
  - Runtime of 9 minutes, recovery time of 13.5 minutes
  - If desired, could be operated with 2 buses and recovery time of 6 minutes
- 2 p.m. to 6 p.m.: service every 10 minutes using 2 buses
  - Runtime of 9 minutes, recovery time of 12 minutes

The BBNR Year One Network **will be updated** to reflect these changes

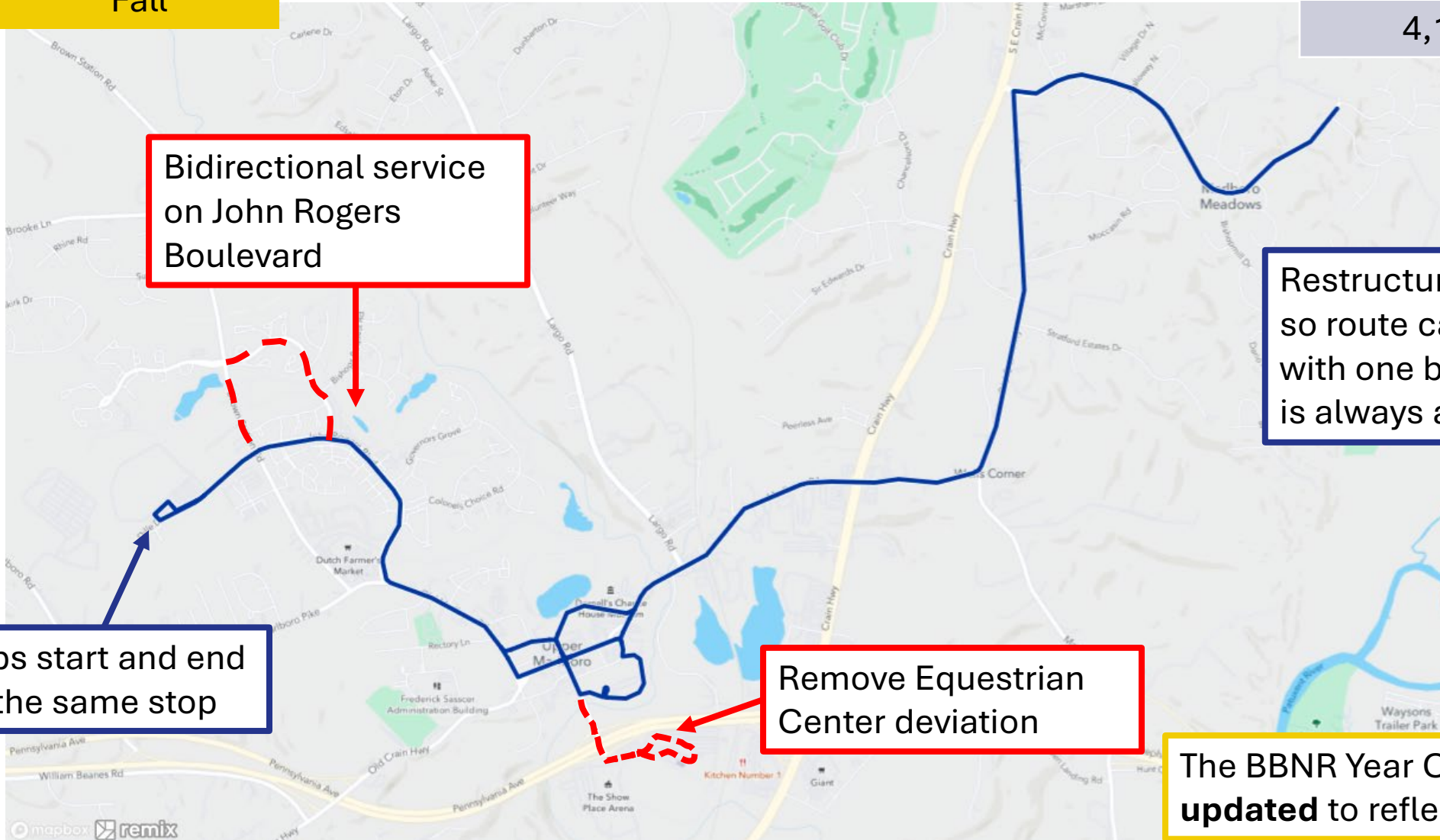
# 53 - Existing



# 53 - Proposed

Fall

Existing Rev Hrs	Proposed Rev Hrs
4,117	2,873



Bidirectional service on John Rogers Boulevard

Restructure schedule so route can be run with one bus (recovery is always at Dille Dr)

Trips start and end at the same stop

Remove Equestrian Center deviation

The BBNR Year One Network **will be updated** to reflect these changes



# Thank you!

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